



Telford & Wrekin  
C O U N C I L

Addenbrooke House Ironmasters Way Telford TF3 4NT

## PLANNING COMMITTEE

Date	<b>Wednesday, 26 April 2017</b>	Time	<b>6.00pm</b>
Venue	<b>Walker Room, Meeting Point House, Southwater Square, Telford TF3 4HS</b>		

### Enquiries Regarding this Agenda:

Democratic Services	Jayne Clarke	(01952) 383205
Media Enquiries	Corporate Communications	(01952) 382406
Lead Officers	David Fletcher – Development Management Service Delivery Manager	(01952) 384133
	Katherine Kynaston – Assistant Director: Business, Development & Employment	(01952) 384012

**Committee Membership:** Councillors J C Minor (**Chairman**), C R Turley (**Vice Chairman**), N A Dugmore, I T W Fletcher, J Loveridge, N C Lowery, L A Murray S J Reynolds and P Scott

**Substitutes:** Councillors G H Cook, J A Francis, E J Greenaway, R T Kiernan, R Mehta, C F Smith, M J Smith and W L Tomlinson

## AGENDA

1. **Apologies for Absence**
2. **Declarations of Interest**
3. **Minutes** Appendix A  
To confirm the minutes of the meeting of the Planning Committee held on 5 April 2017.
4. **Deferred/Withdrawn Applications**
5. **Site Visits**
6. **Planning Applications for Determination** Appendix B  
Please note that the order in which applications are heard may be changed at the meeting. If Members have queries about any of the applications, they are requested to raise them with the relevant Planning Officer prior to the Committee meeting.

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## PLANNING COMMITTEE

Minutes of a meeting of the Planning Committee held on  
Wednesday, 5<sup>th</sup> April 2017 at 6.00pm in the Telford Suite at  
Shropshire College Hotel School Ltd (The Telford Whitehouse Hotel),  
Watling Street, Wellington, Telford TF1 2NJ

**A**

**Present:** Councillors J C Minor (Chair), N A Dugmore, I T W Fletcher, J Loveridge, N C Lowery, L A Murray, P Scott, and C R Turley.

**Also Present:** Councillors A Eade (for planning application TWC/2016/0437)

**PC-067**      **Apologies for Absence**

Councillor S J Reynolds

**PC-068**      **Declarations of Interest**

In respect of planning application TWC/2017/0052, Councillor P Scott advised that he was a member of Newport Town Council but had not been involved in any discussions on this application.

In respect of planning application TWC/2016/1198, Councillor C R Turley advised that he was a member of Hollinswood & Randlay Parish Council but had not been involved in any discussions on this application.

Cllr L A Murray declared an interest in planning application TWC/2017/0105 as he was speaking and indicated that he would withdraw from the meeting during the determination thereof.

**PC-069**      **Minutes**

**RESOLVED** – that the minutes of the meeting of the Planning Committee held on 15 March 2017 be confirmed and signed by the Chairman.

**PC-070**      **Deferred/Withdrawn Applications**

The Chairman informed the Committee that, with regard to planning application TWC/2016/1127, the applicant had requested the application be deferred to address issues of ecology and affordable housing.

**RESOLVED** – that planning applications TWC/2017/0052 be deferred until the meeting of the Committee on 26<sup>th</sup> April 2017 to allow Members to make a Site Visit and that planning application TWC/2016/1127 be deferred for consideration at a later date.

**PC-071**      **Site Visits**

**RESOLVED** – that a site visit takes place at 3.30pm on Wednesday, 26<sup>th</sup> April 2017 at 21 Kings Head Home Park, Newport, Shropshire in respect of planning application TWC/2017/0052.

**PC-072**      **Planning Applications for Determination**

Members had received a schedule of planning applications to be determined by the Committee and fully considered each report.

a) TWC/2016/0437 Land South of junction, A41 / Pave Lane, Newport, Shropshire

This was a full application for a proposed quarry for the extraction of sand and gravel and importation of inert infill material for the restoration of a 51.6 hectare site on land south of the junction of the A41 and Pave Lane, near Newport.

An appeal for non-determination had been sent to the Planning Inspectorate. The purpose of this report was to inform Planning Committee of the recommendation that officers would have made to Planning Committee had the Application been due for determination by them.

The operators were seeking to extract 2.65 million tonnes of sand and gravel over a 15 year operating period. The Traffic Assessment calculated this would lead to 160 HGV movements per day from the site.

The proposals were for the operating hours to be 0700 hours until 1800 hours Monday to Friday and 0700 to 1300 on Saturdays, with no working on Sundays or bank holidays. However, to increase the efficiency of the site, there were proposals for up to 15 HGVs to be loaded following the site closure, parked overnight so the vehicles could leave the site from 0600 hours the following day.

The site would be progressively restored, and the land would be infilled with imported inert materials. This operation would require an environmental permit.

Cllr A Eade, Ward Member, spoke in support of the officer recommendation. Cllr Eade raised concerns regarding the impact of the quarry on the open countryside. Although some screening would be provided, there were concerns regarding the impact on the nearby residential buildings, including Woodcote Wood Nursing Home. Concerns were raised in respect of the impact on the environment, noise, dust, water management and operating hours. Cllr Eade stated that the junction onto the A41 was the worst of its kind in the Borough and it would not be possible to ensure that the junction could be used safely if the proposals went ahead. There was no requirements for the minerals. Cllr Eade stated that the application should be refused as laid out in the officer's report.

Cllr B Harper, Chetwynd Aston and Woodcote Parish Council, spoke in support of the officer recommendation and raised concerns in respect of traffic, operating hours and highways safety. Cllr Harper stated that Woodcote Wood is adjacent to the site and shared the same seam of minerals. Pave Lane was the least preferred site and was not required.

Mrs B Page, Local Resident, spoke in support of the officer recommendation. The local area would be affected by the proposals for more than a generation. No further noise barrier had been proposed, apart from additional trees. There were concerns regarding dust and the impact this would have on nearby residents, particularly those in Woodcote Hall Nursing Home, and concern were raised regarding highways safety, light pollution and impact on local wildlife. Mrs Page stated that the application was the wrong place and would change the character of the countryside.

The Planning Officer stated that there was no need for the mineral, and that the Shropshire and Telford & Wrekin Sand & Gravel landbank adequately exceeds the minimum of 7 years required by the NPPF and Planning Practice Guidance

Woodcote Wood did not currently have full planning permission, however, there was a resolution for approval. Shropshire Council and the new operator were keen to bring Woodcote Wood into fruition. Shropshire Council would need to ratify the decision and there were some issues regarding access to the site, however, representation received by the relevant parties stated these could be resolved. This site was therefore considered deliverable, and would serve the Telford area.

The applicant had not demonstrated that Pave Lane was a better site than any of the allocated or preferred sites in the Minerals Local Plan. The test for a new site to be brought forward over a preferred site was a high one, and the applicants would need to have proven that there were significant environmental benefits for their site. In the case of Pave Lane, the applicant had only demonstrated the minimum requirement for protection and enhancement of biodiversity as set out within NPPF and did not constitute a 'significant environmental benefit' as a result of working the site and as required by the relevant local policies for least preferred sites

There was no need for additional landfill sites within the Borough, there were several other sites which had been mothballed, which were available for use in the same timeframe as this application. Additionally, there had been increased rates of recycling in the Borough, with less inert waste going to landfill.

Due to the level of local concern in respect of Highways, a third party undertook a Road Safety Audit. The Highways Engineer concluded that the junction from the site onto the A41 can cater for HGVs, and that modelling journeys, even at a higher rate than those proposed, would not cause severe harm to the junction.

Environmental Health had completed their assessments of the site and had concluded that the proposed noise levels would be within the acceptable boundaries as outlined in the Planning Practice Guidance.

Members stated that the site was not required and there was no need for the mineral. Members stated that the applicants had not demonstrated that there was a need for this proposal. The proposals were contrary to policy and the site was not one of the preferred sites in the Local Plan.

Some Members raised concerns regarding Highways and requested that Highways Safety be put forward as a reason for refusal. Some Members considered that the junction was high risk and raised their concerns regarding accidents. Some Members considered that not all of the journeys to the proposals had been calculated.

Members considered that the current agricultural use of the land was better suited. Members noted their concerns regarding the impacts on the neighbouring properties, which was felt to be unacceptable.

Upon being put to the vote it was, unanimously:-

**RESOLVED** - that with respect to planning application TWC/2016/0437 that based on the recommendation to the Planning Committee that had an appeal for

non-determination not been submitted that planning permission would have been refused for the following reasons:

**A)**

**1. There is no need for the mineral. Contrary to Policies M15 of the Shropshire and Telford & Wrekin Minerals Local Plan and ER4 of the Telford & Wrekin Local Plan. The Shropshire and Telford & Wrekin Sand & Gravel landbank adequately exceeds the minimum of 7 years required by the NPPF and Planning Practice Guidelines. As a result, the proposal would result in an uncontrolled oversupply of minerals provision.**

**2. The Environmental information submitted with the application does not demonstrate that the site is significantly more acceptable overall than the allocated sites or the preferred area outlined in the Shropshire and Telford & Wrekin Minerals Local Plan, particularly Woodcote Wood which has fewer environmental impacts, a shorter operational period, limited visual impact, less impact on residential amenity and fewer HGV movements. Nor would it offer significant environmental benefits. As such, it would conflict with save policies M14 and M15 of the Shropshire and Telford & Wrekin Minerals Local Plan; and Policies SP3, SP4, ER4 and ER6 of the Telford & Wrekin Local Plan (Submission Version) and national planning policy.**

**3. The need for the waste facility as an additional landfill site has not been established and any purported benefits arising from the proposal are significantly outweighed by the loss of best and most versatile agricultural land. As such, the proposal is contrary to Policy ER7 in the Telford & Wrekin Local Plan (Submission Version) and national planning policy.**

**4. The potential cumulative impact of working both the Woodcote Wood and Pave Lane sites concurrently is not considered sustainable and is contrary to national guidance (para 144 NPPF), policy ER6 of the Telford & Wrekin Local Plan (Submission Version) and save policy M3 of the Shropshire and Telford & Wrekin Minerals Local Plan and national planning policy.**

**B) In order to give officers flexibility during this appeal, that the Development Management Service Delivery Manager or any other officer authorised by him, be authorised to add to, change or amend the refusal reasons.**

**C) For the purposes of the appeal only that we have authority to enter into an obligation under section 106 of the 1990 Act to secure the necessary obligations from the applicant in the event that the appeal is allowed.**

b) TWC/2016/1198 Plot 2 & 3 Telford 54 Business Park, Nedge Hill, Telford, Shropshire

This was a full application for the erection of a 12,699 square meter industrial building (use class B1, B2 and B8) and associated car parking. Planning permission had previously been granted for in 2016 for two industrial buildings on the site, however, an end user had not been identified at the time of the original application.

The proposals would create 240 jobs in the area. Financial contributions towards improvements on highways and bus stops in the vicinity had been requested as part of a S.106 agreement with the applicant.

An updated report had been provided to include the Planning Ecologist's comments, which noted that only minor amendments would be needed to the existing EPS mitigation license (great crested newts), and they were satisfied that the proposals would not be detrimental to the population of great crested newts in the area.

The Members supported the proposals, which would create new jobs for Telford and was an excellent use of land which had been designated for employment.

Upon being put to the vote it was, unanimously:-

**RESOLVED** – that with respect to planning application TWC/2016/1127 that delegated authority be granted to the Development Management Service Delivery Manager to grant planning permission subject to the following:

- A) The applicant / landowners entering into a Section 106 agreement with the Local Planning Authority relating to:**
- (i) A financial contribution of £72,038.31 towards improvements to the local highway network in the vicinity of the application site, and**
  - (ii) A bus stop contribution of £10,000 towards improvements to the existing bus stops within the vicinity of the application site**
- B) The conditions and informatives set out in the update report (with authority to finalise conditions and reasons for approval to be delegated to the Development Management Service Delivery Manager).**
- c) TWC/2017/0105 Garages between. 129 & 131 Crescent Road, Hadley, Telford, Shropshire

*In accordance with his declaration of interest Councillor L A Murray left the room during determination of the following application, after speaking against the application in his capacity as Ward Member.*

This application was a full application seeking consent for the erection of a two storey building, containing 4 two bedroom flats, taking the appearance of a pair of semi-detached houses, following the demolition of ten garages.

Outline consent for two bungalows and ten garages had been granted in 2008 at the site. The existing garages are in a state of disrepair and officers consider this a sensible regeneration scheme.

Councillor L Murray, Ward Member, spoke against the proposals. The plot has been disused for some time, however, there have been twenty objections from local residents to the scheme. Cllr Murray stated that there were concerns regarding overlooking and access to the site, which was narrow. The development was inappropriate and the site was not large enough for the proposals. The land would be better used as an area for residents, such as a children's play area.

The Planning Officer stated that ten letters of objection had been received, and a 20 signature petition, raising concerns regarding loss of light, noise and the limited capacity of local schools.

The proposed dwellings were located 15m from the boundary of the site, with only bathroom windows on the aspect towards neighbouring properties, glazing to these windows would be conditioned.

The Highways Officer has no objections to the proposals. It was considered that the existing ten garages would produce a similar amount of traffic to the proposals. Although the access was narrower than adoptable standards for a new development, these did not apply as the access had already been in use.

A Member considered that the access to the site was too narrow and that the demolition of the garages would reduce parking in the area. A Member believed that there was a public right of way through the site.

Other Members considered that the majority of the garages were not in use, so the proposals would not result in the loss of parking. The principle of development had been established by the previous planning permission. Although the access was narrow, Members noted that the access had been used for the garages. Some Members considered that while the access was not ideal, the proposal was acceptable on balance.

Some Members considered that flats were needed in the area and similar applications had been approved around the Borough.

Upon being put to the vote it was, by majority:-

**RESOLVED** – that with respect to planning application TWC/2017/0105 that delegated authority be granted to the Development Management Service Delivery Manager to grant planning permission subject to the conditions and informatives set out in the report (with authority to finalise conditions and reasons for approval to be delegated to the Development Management Service Delivery Manager).

The meeting ended at 6.59 pm

**Chairman:** .....

**Date:** .....

TELFORD & WREKIN COUNCIL



PLANNING COMMITTEE

26 April 2017

Schedule 1 - Planning applications for determination by Planning Committee

**TWC/2017/0052 21 Kings Head Home Park, Newport, Shropshire, TF10 7LG**

Erection of a single storey rear extension and installation of a decking area

\*\*\*\*Updated Location & Block Plan Received\*\*\*\* ..... 11

**TWC/2017/0208 Foot Bridge to Telford Central Railway Station, Telford,**

**Shropshire** Minor amendment to planning permission reference TWC/2016/0641

(Erection of replacement footbridge following demolition and removal of existing bridge structure together with associated level alterations, provision of retaining structures and access alterations) to omit pedestrian access ramp and supporting earthworks and amend the design of remaining ramps, staircases and supporting structures ..... 23

## PLANNING COMMITTEE LIST OF BACKGROUND PAPERS

The Background Papers taken into account when considering planning applications on this list include all or some of the following items. Items 1 to 4 are included on the file for each individual application.

1. Application: includes the application form, certificate under Section 65 of the Town and Country Planning Act, 1990, plans, and any further supporting information submitted with the application.
2. Further correspondence with applicant: includes any amendments to the application – including any letters to the applicant/agent with respect to the application and any further correspondence submitted by the applicant/agent, together with any revised details and/or plans.
3. Letters from Statutory Bodies: includes any relevant letters to and from the Parish Councils, Departments of Telford & Wrekin Council, Water Authorities and other public bodies and societies.
4. Letters from Private Individuals: includes any relevant letters to and from members of the public with respect to the application, unless the writers have asked that their views are not reported publicly.
5. Statutory Plans and Informal Policy Documents: some or all of the following documents will comprise general background papers taken into account in considering planning applications in the administrative area of Telford and Wrekin (“Telford and Wrekin”)
  - (a) The Telford and Wrekin LDF Core Strategy (adopted December 2007)
  - (b) Saved policies of Wrekin Local Plan (adopted February 2000)
  - (c) Saved policies of Shropshire and Telford and Wrekin Joint Structure Plan (adopted November 2002)
  - (d) Saved policies of Shropshire, Telford and Wrekin Minerals Local Plan (adopted April 2000)
  - (e) Government Planning Guidance – National Planning Policy Framework and Circulars
  - (f) Town and Country Planning legislation, case law and other planning decisions and articles
  - (g) Telford and Wrekin Supplementary Planning Documents: Design for Community Safety SPD (adopted June 2008) and Telecommunications Development SPD (adopted May 2009)
  - (h) LDF Central Telford Area Action Plan (adopted March 2011 )
6. Past decision notices and reports referred to in specific reports.
7. The following additional documents (if appropriate):-

TWC/2017/0052

21 Kings Head Home Park, Newport, Shropshire, TF10 7LG

Erection of a single storey rear extension and installation of a decking area

\*\*\*\*\*Updated Location & Block Plan Received\*\*\*\*\*

**APPLICANT**

Michael Wemm

**RECEIVED**

24/01/2017

**PARISH**

Newport

**WARD**

Newport North and West

**OFFICER** Kirsty Johnson

**CLLR TIM NELSON HAS REQUESTED THE APPLICATION IS CONSIDERED BY MEMBERS OF COMMITTEE**

**INTRODUCTION**

This application was deferred at the Planning Committee held on the 5<sup>th</sup> April 2017 to allow Members to visit the site prior to the subsequent planning committee.

**1. THE PROPOSAL**

- 1.1 The application seeks consent for the erection of a single storey extension to the north elevation of an existing park home at 21 Kings Head Park, Newport. The proposal also includes an area of decking to the west elevation.
- 1.2 The single storey extension will measure 3.4m x 3.4m and will match the eave and ridge height of the existing park home. The extension has no windows on the north elevation, a long top opening window on the west and a three pane patio door on the east elevation. The extension will be made to match the existing park home.
- 1.3 The decking will extend 1.2m from the west elevation with a length of 2.7m. It will be built on brickwork base with a stainless steel and glass balustrade around the edge.

**2. SITE AND SURROUNDINGS**

- 2.1 The application site is located in the built up area of Newport, accessed from Chetwynd End and existed from Green Lane. The site comprises of residential park homes located to the rear of 'TFM Farm & Country Superstore'.
- 2.2 To the north of the application site is a recent residential development site providing 85 dwellings on land between Kings Head Park and Edgmond Road (TWC/2014/0401). The Kings Head Caravan Park originally had 15 mobile homes and has been increased by a further 13 mobile homes to replace 9 touring caravans that were previously using the land. 21 Kings Head Park

forms part of this recent extension (TWC/2014/0036) to the park home site that has extended the original park home further west along Green Lane.

- 2.3 21 Kings Head Park is a single storey park home cited lengthways along the northern boundary of the park home site. The park home is clad with a gable pitched roof. The existing park home is 4.3m high a width of 6m and a length of 14m. As the park home is not sited parallel to the northern boundary it is situated between 7.9m - 9.3m from the boundary.
- 2.4 To the east of the application site is no. 20 Kings Head Park and to the west is a vacant park home plot. To the north are no. 33, 35,37 & 37 Stone Bridge which are two storey dwellings located approximately 21m from the existing rear wall of the 21 Kings Head Park. There are well established trees screening along the northern boundary. There is a gap in this run of trees and a replacement tree has been planted in this space between 21 Kings Head Park & 37 Stone Bridge.

### **3. RELEVANT PLANNING HISTORY**

- 3.1 Original Consent: TWC/2014/0036 - Change of use of land to caravan site: Kings Head Mobile Home Park, Green Lane, Newport, Shropshire, Decision: Full Granted:16/05/2014
- 3.2 Recent approval next to application site: TWC/2016/1150 - Erection of a conservatory: 20 Kings Head Park, Green Lane, Newport, Shropshire, TF10 7LG:Full Granted:07/02/2017
- 3.3 Adjacent Edgmond Road Site Consent: TWC/2014/0401- Erection of 85 dwellings with associated access and landscaping (Reserved Matters Application)\*\*\*Amended Details Received\*\*\* - Reserved Matters Granted – 28/08/2014

### **4. PLANNING POLICY CONTEXT**

- 4.1 National Planning Policy Framework (the NPPF)
- 4.2 Saved Wrekin Local Plan policies  
UD2 Design Criteria
- 4.3 LDF Core Strategy policies  
CS15 Urban Design
- 4.4 Submission Version Telford & Wrekin Local Plan  
BE1 Design Criteria  
BE2 Residential Alterations

### **5. SUMMARY OF CONSULTATION RESPONSES**

- 5.1 Newport Town Council: No objection

- 5.2 Cllr Tim Nelson: Object – overbearing impact on 37 Stone Bridge, inaccuracy of plans
- 5.3 Shropshire Fire Service: No comment
- 5.4 Neighbour Consultation: Notification letters were sent to the occupiers of six neighbouring properties. In response four letters of objection have been received raising the following issues:
- Inaccuracy of site plan/location plan
  - Gable end will be imposing on residents
  - Level difference
  - Additional water run off/ site drainage issues
  - Lack of information –section through site
  - Land stability
  - Extension will be intrusive, result in loss of light, overbearing
  - Concerns regarding the distance from boundary to residents from Stone Bridge.
  - Impact on resale of nearby properties

## **6. PLANNING CONSIDERATIONS**

- 6.1 Having regard to the development plan policy and other material considerations including comments received during the consultation process, the planning application raises the following main issues:
- The design, mass, scale and appearance
  - The impact on the living conditions of neighbours

### The design, mass, scale and appearance

- 6.2 Policy CS15 (Urban Design) of the Core Strategy requires development to positively influence the appearance and use of the local environment.
- 6.3 Policy UD2 (Design Criteria) of the Wrekin Local Plan supports development which respects and responds positively to its context and where appropriate provides distinctive design. This policy also supports development which respects and relates to any adjacent building and features.
- 6.4 Telford & Wrekin Local Plan (Submission Version) policies BE1 (Design Criteria) and BE2 (Residential Alterations) reinforce the above and also advise that development should be proportionate in size in relation to the existing plot, respect the character of area, be in keeping with the existing building and be adequately accommodated within its curtilage without adversely affecting its amenity or that of neighbouring properties.
- 6.5 Officers are satisfied that the scale of the extensions are proportionate to the size of the existing park home with a modest 1.2m x 2.7m platform and 3.4m x 3.4m extension with the eaves height (3m) and ridge height (4.3) to match the existing.

- 6.6 The extension will respect the character of the area and be in keeping as the design and appearance of the extension will match the existing park home. The extension can be adequately accommodated within its curtilage with a sufficient amount of garden space retained and a separation distance of 5.3m can be achieved from the extension and the site boundary. As such the proposal will not result in overdevelopment of the site. The application is therefore considered to be compliant with CS15 of the Core Strategy, UD2 of the Wrekin Local Plan and BE1 & BE2 of the emerging Telford & Wrekin Local Plan in this regard.

The impact on the living conditions of neighbours

- 6.7 37 Stone Bridge is located approximately 21m north of 21 Kings Head Park. There is a noticeable level difference between this property and the application site with the park home siting higher than 37 Stone Bridge. The longest side of the park home which fronts this property is mainly screened by established trees with a small portion (approximately 3m) of the park home being visible due to the removal of a dead tree along the boundary which divides the plots, notably the site owners have replanted a tree in its place. In addition the timber boundary fence masks the lower part of the park home further restricting its view.
- 6.8 The extension will result in occupants of no. 37 being able to see a gable end at the same height of the existing park home with no windows in the facing elevation. A distance separation of approximately 17.6m between the rear wall of the extended park home and 37 Stone Bridge will be provided.
- 6.9 Having visited no. 37 and stood in this residents garden, officers are satisfied that there are no issues of overlooking due to the lack of windows in the elevation facing 37 Stone Bridge and whilst it is acknowledged that there is a level difference and the extension will be visible, the current boundary treatment and distance separation will ensure the development is not overbearing on this neighbour.
- 6.10 No. 35 & 33 Stone Bridge are located approximately 21m north of 21 Kings Head Park, again there is a noticeable level difference, however, the park home site is well screened to the occupants of these properties with well-established boundary trees. The orientation of the dwellings to the park home and the separation distance will ensure there are no issues of overlooking or loss of light and in addition, the development will not be overbearing on these residents.
- 6.11 The extension will not impact on the future occupants of the park home plot to the west with a high level window limiting opportunities for overlooking.
- 6.12 The locations of the extension and decked area are on elevations which do not face 20 Kings Head Park as such the development will not be overbearing, result in loss of light or privacy.

- 6.13 The proposed decking will allow the occupants of 21 Kings Head Park to stand approximately 0.55m above ground level and whilst there may be some issues of overlooking, given the scale of the structure with a depth of 1.2m, it is unlikely to be used as amenity space and instead is more likely to be for access to the raised patio doors limiting opportunities for overlooking. In addition officers recognised that removable steps to the park home could be placed in this position without the need for planning permission.

#### Other matters

- 6.14 Comments have been received with regards to the accuracy of the plans. Amended site/location plans have been provided which show the accurate position of the park home; officers have measured the distance between the site boundary and the park home on site and are in agreement with the submitted plans.
- 6.15 A request has been made by residents for an additional plan to show a section through the site. Whilst officers appreciate there is a level difference, given the distance separation and the low level form of development, in this instance it is not considered necessary for a section plan to be provided.
- 6.16 Whilst officers appreciate comments made in relation to additional run off water associated with this development, officers have approached the Council's Drainage Engineers who have advised they have no comment to make. Officers also consider any additional request for drainage details would be unreasonable given the scale of the development. Furthermore, a similar application has been approved at no. 20 (TWC/2016/1150) and it was not considered necessary for drainage details to be submitted.
- 6.17 There are no known issues of land instability on this site and therefore details of land stabilisation are not required for this application.
- 6.18 The consent for the park home site (TWC/2014/0036) does not control the site layout and as such resident's comments with regards to an 'agreed distance from the boundary' apply to those imposed by the site licence and are not controlled or enforceable by planning conditions.
- 6.19 Comments with regards to impact on property value are not a material planning consideration.
- 6.20 All other objections raised in the consultation process have been considered but do not raise any issues that would warrant a review of the analysis of this proposal.

## **7. CONCLUSIONS**

- 7.1 The extension and narrow decked area can be adequately accommodated in the site with an appropriately designed scheme proportionate to the existing park home. Whilst officers appreciate that the park home will be closer to neighbouring properties this is not considered to cause an unacceptable

impact on the living standards due to the separation distance and scale and design of the proposals. The development will not be overbearing, or result in issues of loss of light or privacy and will therefore not result in any negative impact to the amenity of neighbours. Therefore the application is considered to be compliant with UD2 of the Wrekin Local Plan, CS15 of the Core Strategy and BE1 & BE2 of the emerging Telford & Wrekin Local Plan.

## **8. RECOMMENDATION**

8.1 Based on the conclusions above, it is recommended to **GRANT PLANNING PERMISSION** subject to the following conditions:

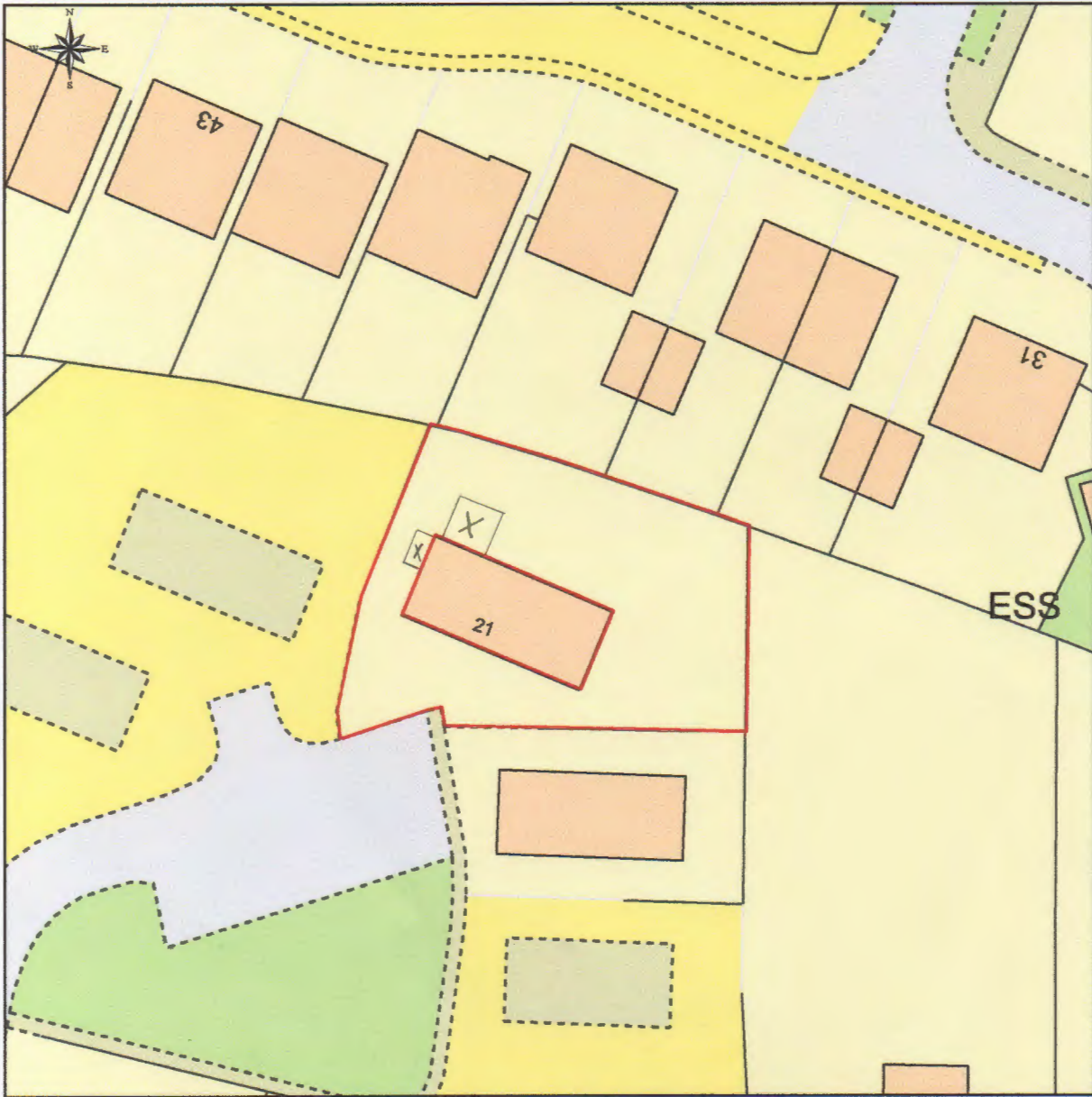
1. A04 – Time Limit
2. C001 – Materials to match
3. C38 – Development in accordance with deposited plans

### Informatives

licustom – License and Caravan Site and Control of Development Act 1960



# Block Plan of 21 Kings Head Home Park, NEWPORT, TF10 7LG



This Plan includes the following Licensed Data: OS MasterMap Colour PDF Site Plan/Block Plan by the Ordnance Survey National Geographic Database and incorporating surveyed revision available at the date of production. Reproduction in whole or in part is prohibited without the prior permission of Ordnance Survey. The representation of a road, track or path is no evidence of a right of way. The representation of features, as lines is no evidence of a property boundary. © Crown copyright and database rights, 2017. Ordnance Survey 0100031673

0m 5m 10m 15m 20m 25m 30m 35m 40m

Scale: 1:500, paper size: A4

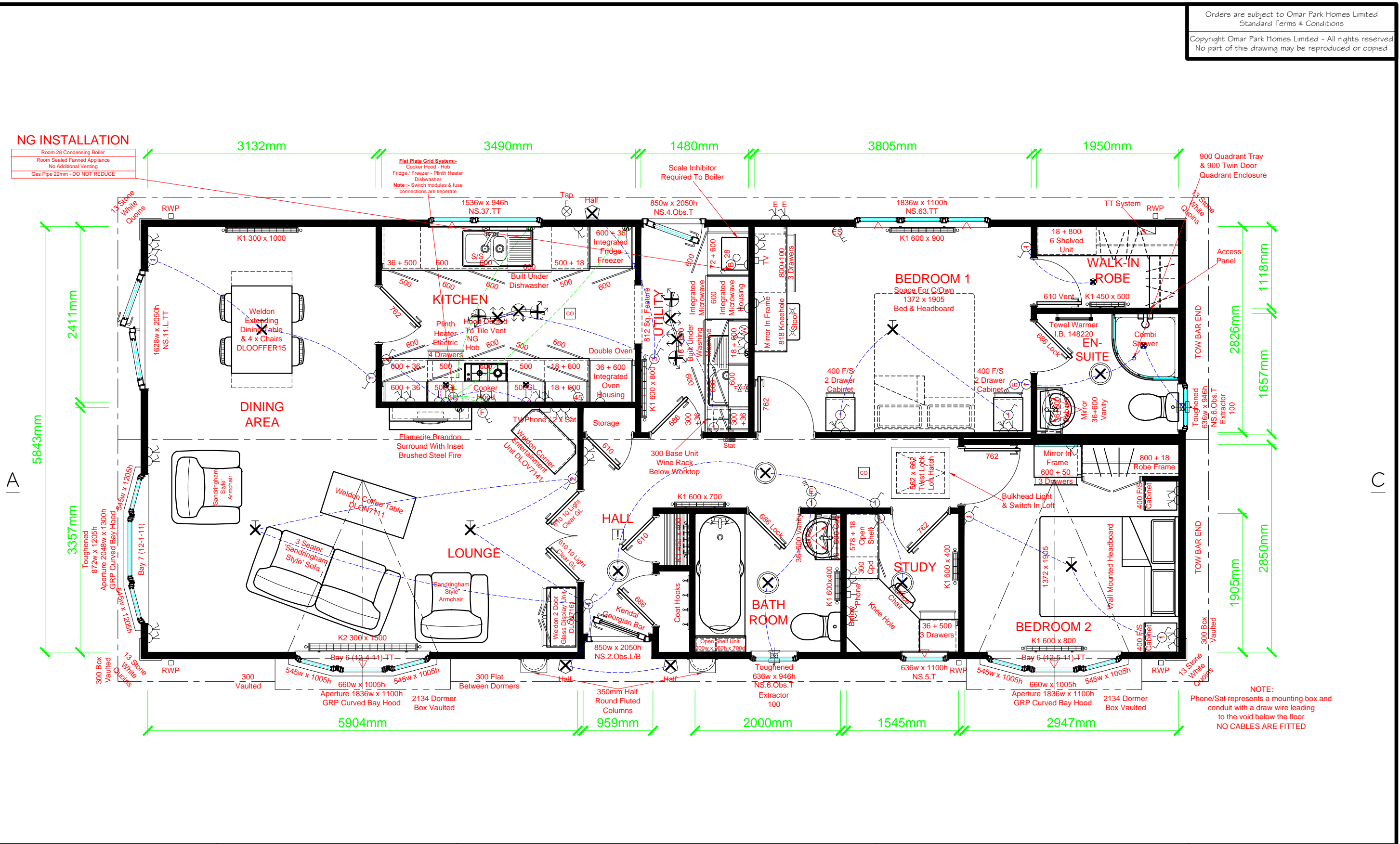


ashwoodoffice interiors  
100% OFFICE REDESIGNERS

plans ahead by emapsite

B

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London Road, Brandon, Suffolk. IP27 ONE  
Phone: 01842 810673 - Fax: 01842 814328

PARK OWNER: APPROXIMATE DIMENSIONS  
Please note that the following overhang dimensions will need to be considered in order to comply with the spacing requirements of the 'Model Standards - 1989' on your park

Overall Wall Length Including Cladding = 13.774m  
Overall Wall Width Including Cladding = 6.070m  
Overall Wall Length Including Roof & Windows = 14.370m  
Overall Wall Width Including Roof & Windows = 7.028m  
Maximum Transport Width = 3.564m  
Maximum Transport Height = 4.350m

14.326m x 6.012m (47'-0" x 20')

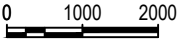
Middleton 2DB Utility & Study

Kingshead Park

No:	Revision:	Date:
A	Production details added. (MRE 1207G)	24-12-15
B	Production details amended. (AGP 12129)	08-01-16

Date:	25-11-15	11845
Scale:	1:50	
Drawn By:	BJE	
Drawing No:	38 15 1096	
Serial No:	16 OM 38 6142	
This home is designed in accordance with B5.3632		

D



Scale  
1:100  
@ A3

Client : **M Wemm**  
21 Kings Head Home Park  
NEWPORT, TF10 7LG

Drawing Title: 21 Kings Head Home Park Extension  
Existing Elevation

Drawn by : **Ashwood Office Interiors Ltd**  
Unit 3, Spring Cottage Industrial Estate  
Neachells Lane, WILLENHALL, WV13 3RG

Date  
19 Jan  
2017

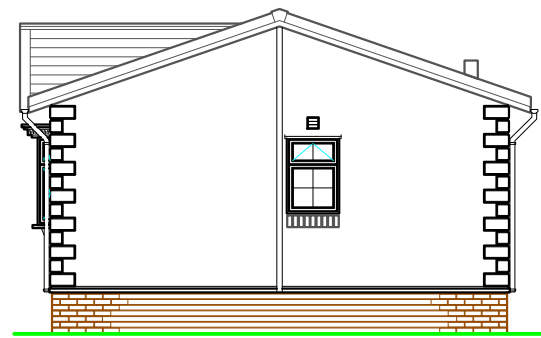
DWG No  
---/3



Front End Elevation



Nearside Elevation



Rear End Elevation



Offside Elevation

## SPECIFICATION

0 500 1000

Scale  
1:50  
@ A3

### SUBFRAME

3No. 100x65x7mm Channels with cold rolled steel joists positioned across channels @ 1220mm c/c

Which supports exterior walls along the perimeter supported by levelling jack.

### FLOOR

High density 18mm thick flooring grade T&G chipboard screwed & nailed to 96x45mm stress graded timber joists at a maximum 407mm/c which are securely bolted to the subframe at regular intervals. The floor deck is insulated with platinum grade rigid polystyrene which is held in position between the joists with a continuous 125 micron, 20% fire retardant moisture barrier to BS.2782. The average floor 'u' value which includes floor joists is 0.37W/m2/K.

### WALLS

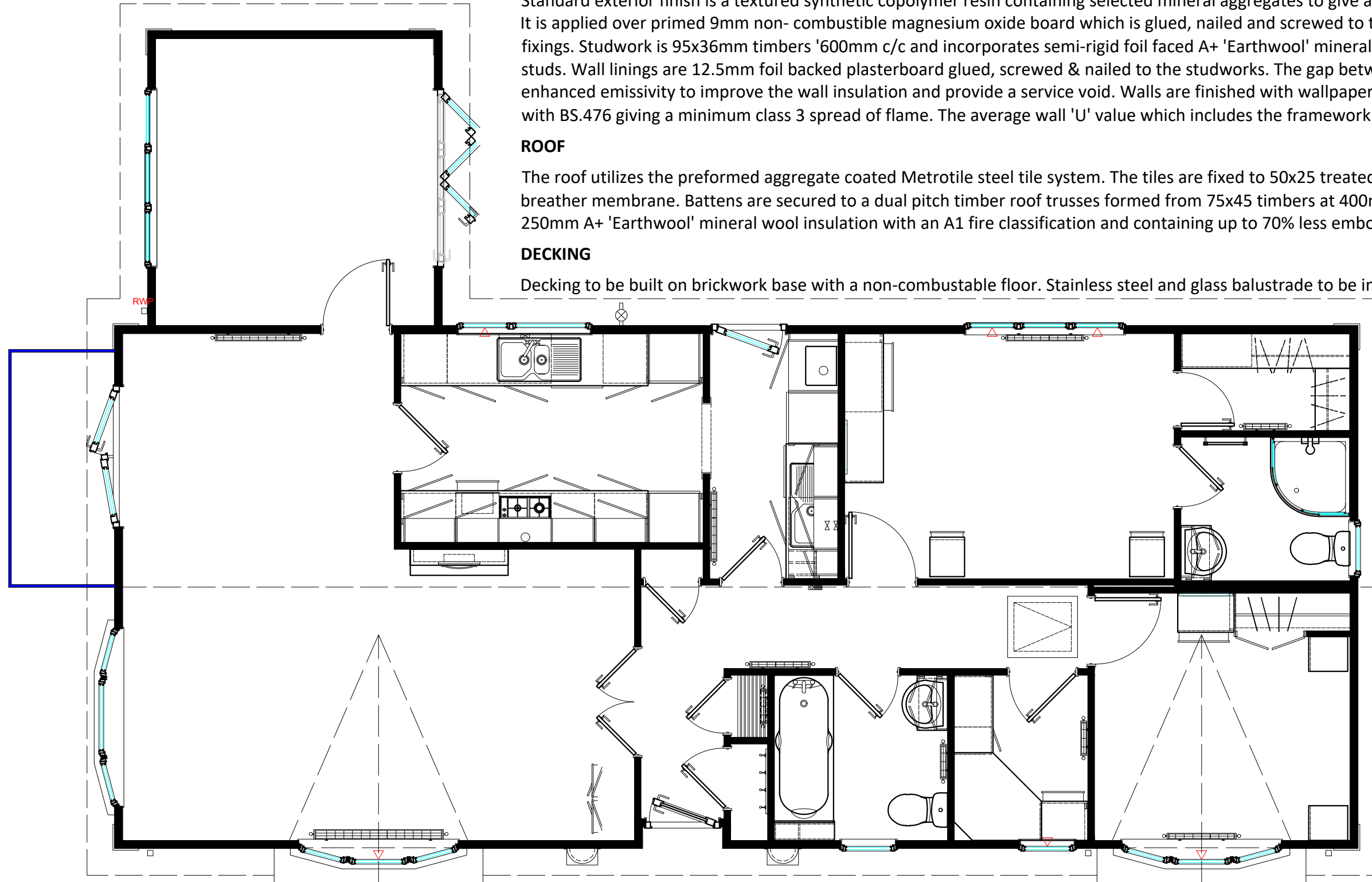
Standard exterior finish is a textured synthetic copolymer resin containing selected mineral aggregates to give a class 1 spread of flame to BS.476. It is applied over primed 9mm non-combustible magnesium oxide board which is glued, nailed and screwed to the studwork with stainless steel fixings. Studwork is 95x36mm timbers '600mm c/c and incorporates semi-rigid foil faced A+ 'Earthwool' mineral wool insulation between the studs. Wall linings are 12.5mm foil backed plasterboard glued, screwed & nailed to the studworks. The gap between the opposing foil faces gives enhanced emissivity to improve the wall insulation and provide a service void. Walls are finished with wallpaper. All internal substrates comply with BS.476 giving a minimum class 3 spread of flame. The average wall 'U' value which includes the framework is better than 0.46 W/m2/K.

### ROOF

The roof utilizes the preformed aggregate coated Metrotile steel tile system. The tiles are fixed to 50x25 treated timber battens over Corovin breather membrane. Battens are secured to a dual pitch timber roof trusses formed from 75x45 timbers at 400mm c/c. The roof is insulated with 250mm A+ 'Earthwool' mineral wool insulation with an A1 fire classification and containing up to 70% less embodied.

### DECKING

Decking to be built on brickwork base with a non-combustable floor. Stainless steel and glass balustrade to be installed around the edge



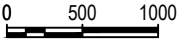
Client: M Wemm  
21 Kings Head Home Park  
NEWPORT, TF10 7LG

Drawing Title: 21 Kings Head Home Park Extension  
Proposed Plan

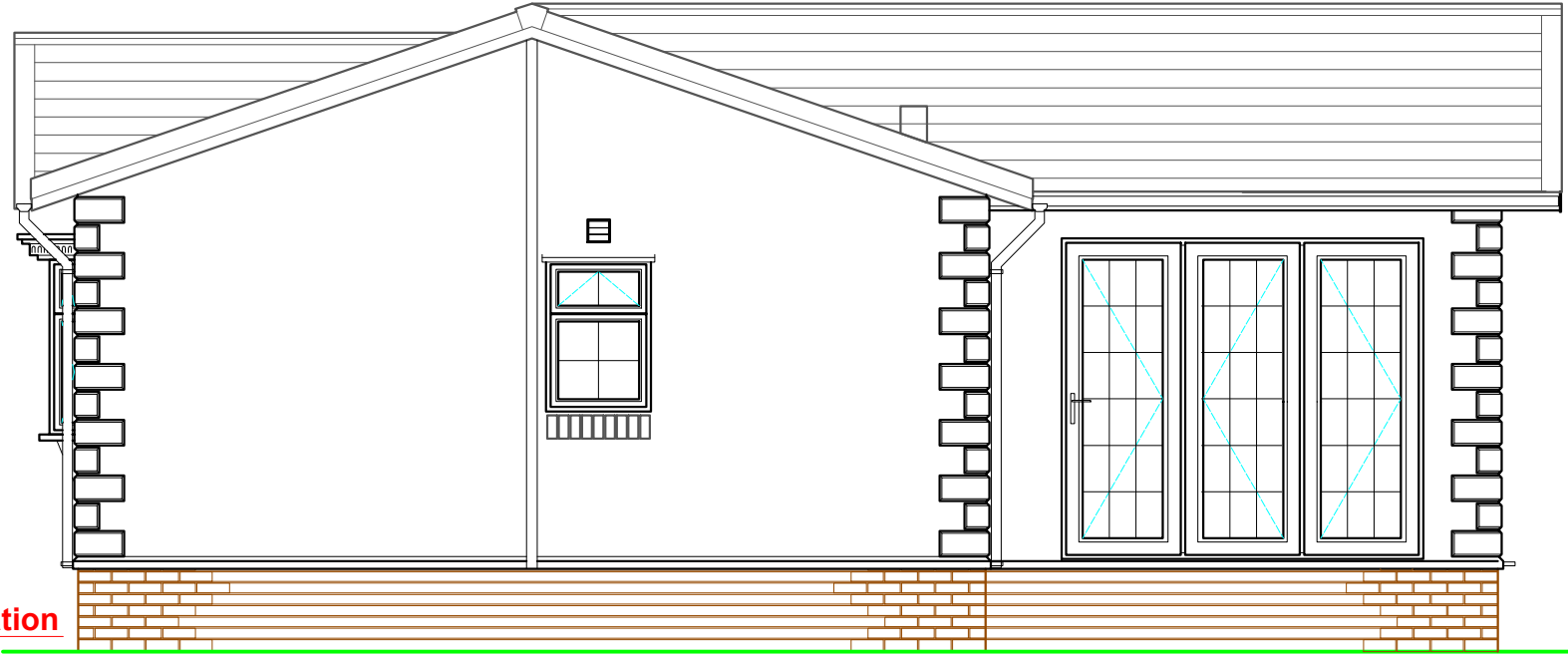
Drawn by: Ashwood Office Interiors Ltd  
Unit 3, Spring Cottage Industrial Estate  
Neachells Lane, WILLENHALL, WV13 3RG

Date  
5 Dec  
2016

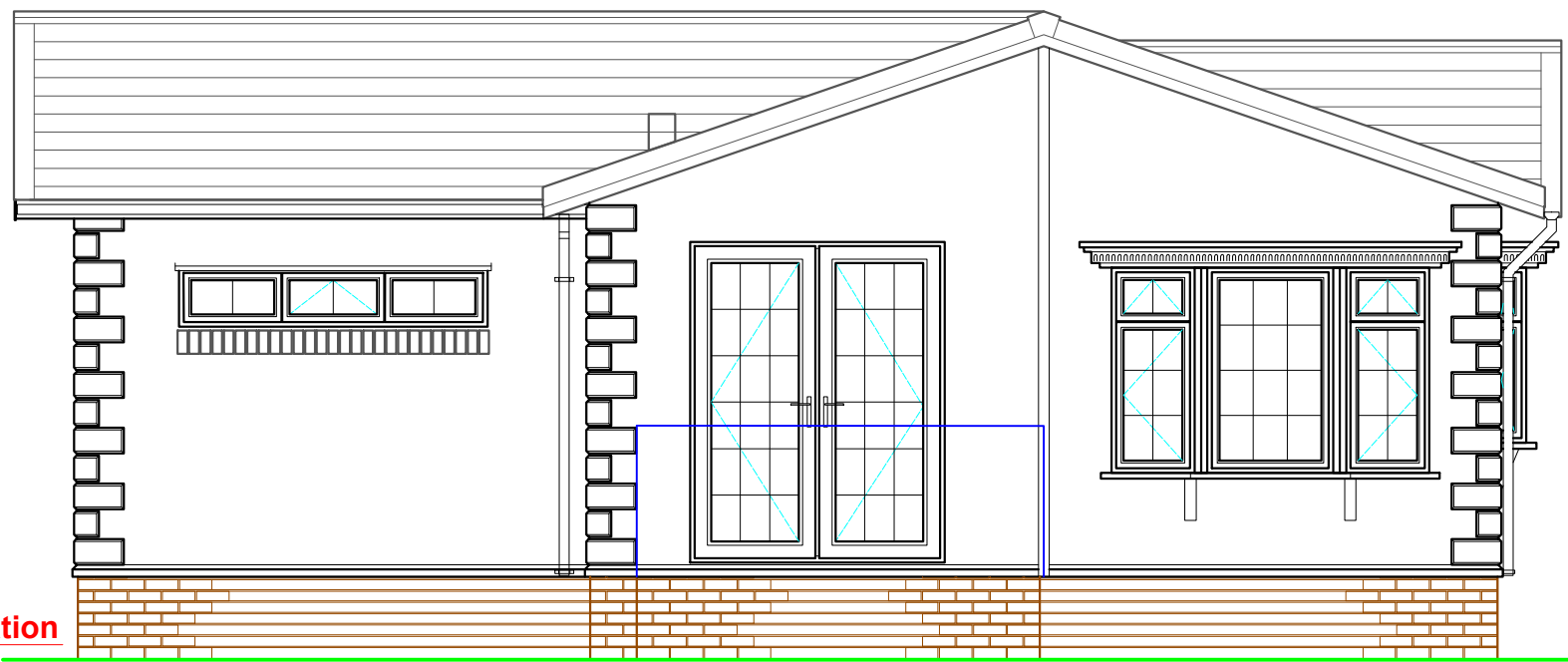
DWG No  
---/1



Scale  
1:50  
@ A3



Rear End Elevation



Front End Elevation



Offside Elevation

Client : **M Wemm**  
21 Kings Head Home Park  
NEWPORT, TF10 7LG

Drawing Title: 21 Kings Head Home Park Extension  
Proposed Elevation

Drawn by : **Ashwood Office Interiors Ltd**  
Unit 3, Spring Cottage Industrial Estate  
Neachells Lane, WILLENHALL, WV13 3RG

Date  
5 Dec  
2016

DWG No  
---/2

TWC/2017/0208

Foot Bridge to Telford Central Railway Station, Telford, Shropshire

Minor amendment to planning permission reference TWC/2016/0641 (Erection of replacement footbridge following demolition and removal of existing bridge structure together with associated level alterations, provision of retaining structures and access alterations) to omit pedestrian access ramp and supporting earthworks and amend the design of remaining ramps, staircases and supporting structures

**APPLICANT**

Telford and Wrekin Council

**RECEIVED**

13/03/2017

**PARISH**

Lawley and Overdale

**WARD**

Ketley and Overdale

**OFFICER** Steven Drury

**1.0 PROPOSAL**

- 1.1 This application seeks minor material amendments to existing planning permission reference TWC/2016/0641 which granted permission for the erection of a replacement footbridge, retaining structures, ramps, walkways and associated infrastructure, following the demolition and removal of the existing footbridge.
- 1.2 The proposed amendments will involve changes to the access provisions to the bridge deck from both platforms at Telford Central Railway Station. More specifically the amendments will consist of the following: -
  - Omission of pedestrian access ramp (and supporting earthworks) linking the bridge deck to westbound platform 2,
  - Amendments to the staircase position and design linking the bridge deck to platforms on both sides of the railway line,
  - Ramped access to platform 1 to be provided through retention of existing ramped approach which will be connected to the new bridge via a 31m steel ramp on piers.
- 1.3 The amendments proposed are sought as a result of increasing construction costs arising due to factors such as poor ground conditions under the earthwork ramps which require additional piling and groundworks. Limited funding is available for the project and the applicant has therefore sought to explore ways to minimise costs without compromising bridge design and concept.
- 1.4 The remainder of the scheme will be as per the approved application under TWC/2016/641.
- 1.5 The proposed replacement bridge will be erected alongside the existing footbridge which will remain in operation until the new bridge is completed and brought into use.

## **2.0 SITE AND SURROUNDINGS**

- 2.1 The site is located on the north eastern edge of Telford Town Centre immediately to the south of the M54 and to the north east of a zone of office and commercial development to the north of the town centre. The existing bridge currently provides a pedestrian link from Telford Central Railway Station to the town centre via Ironmasters Way and Lawn Central. The bridge spans the two track railway line, the A442 Queensway and A5 Rampart Way.
- 2.2 Land immediately to the north is woodland which forms a green buffer alongside the M54 motorway. Land to the east comprises Telford Central Station, station car parking, Euston Way pub and Premier Inn hotel. To the west of Rampart Way is commercial development and Staples retail unit to the south subject to an approved application to subdivide into three retail units and a gymnasium under TWC/2016/0617.
- 2.3 Despite being well enclosed on both side by rising levels and well treed areas, the bridge is highly visible on approach from the north and south and is visible in passing from the M54. The bridge is also well used as the pedestrian link to the town centre for people arriving at Telford Central Station by train and is also part of National Cycle Route (NCR) 55.

## **3.0 RELEVANT PLANNING HISTORY**

- 3.1 TWC/2016/0641 - Erection of replacement footbridge following demolition and removal of existing bridge structure together with associated level alterations, provision of retaining structures and access alterations – Granted 20<sup>th</sup> October 2016

## **4.0 PLANNING POLICY CONTEXT**

### **4.1.1 Core Strategy:**

CS3: Telford  
CS4: Central Telford  
CS9: Accessibility and Social Inclusion  
CS12: Natural Environment  
CS13: Environmental Resources  
CS14: Cultural, Historic and Built Environment  
CS15: Urban Design

### **4.1.2 Wrekin Local Plan:**

UD2: Design Criteria  
UD3: Urban Design Assessments  
UD4: Landscape Design  
TC1: Town Centre  
TC14: Town Centre Design

### **4.1.3 Central Telford Area Action Plan (CTAAP):**

TC7: Telford Gateway  
CT13: Pedestrian and Cycle Network

CT15: Design  
CT17: Public Realm  
CT19: Biodiversity  
CT20: Landscape

4.1.4 Telford & Wrekin Local Plan (Submission Version June 2016)

EC5: Telford Town Centre  
BE1: Design Criteria

## 5.0 SUMMARY OF CONSULTATION RESPONSES

### 5.1 Parish/Town Council Response

5.1.1 Lawley and Overdale Parish Council: No comment

### 5.2 Standard consultation responses

5.2.1 Drainage: No comment

5.2.2 Environmental Health (Pollution Control): No comment

5.2.3 Environmental Health (Contaminated Land): No comment

5.2.4 Arboricultural: Support

5.2.5 Public Rights of Way: No objection

Having looked at the plans and the strategic path network, I am satisfied that the current design of the proposed bridge, along with the proposed ground level routes, will not disadvantage any category of rights of way user. The provision of lifts at the Station end of the bridge will be an improved facility for wheelchair/pushchair users who currently are obliged to negotiate a ramp with a steeper than recommended gradient.

5.2.6 Ecology: Comment

No objection in principle. Recommends conditions and informatives to be added to any decision notice. Requests further clarification on areas subject to tree removal and potential requirement for preliminary roosting surveys to be carried out in extended temporary laydown areas.

5.2.7 Highways England: No objection

Confirm response remains the same to the initial proposal sent July 28 2016 - have no additional comments to make.

5.2.8 Network Rail: No comment

Network Rail has no comments to make.

5.2.9 Shropshire Fire Service: No comment

### **5.3 Public consultation responses**

5.3.1 One comment received from a member of the public raising the following points: -

- 1) Questions what happens when the lift on the Westbound platform is out of commission. When it is, there will be absolutely NO disabled access to and from the Westbound platform.
- 2) There is no need to close and stop up the layby along the A442 which is heavily used at present. This would allow vehicle access to drop off or collect a disabled person in case the lift is out of commission.
- 3) Please reconsider the use of a GRP soffit to the bridge span. This will be the first part of the bridge to start to deteriorate and look shabby.

### **6.0 PLANNING CONSIDERATIONS**

6.1 Having regard to the development plan policy and other material considerations including comments received during the consultation process, the planning application raises the following main issues:

- Principle of Development
- Visual Impact
- Access Issues
- Trees and Ecology Issues

#### **6.2 Principle of Development**

6.2.1 The principle of the development has been established through granting of TWC/2016/0641 on 20th October 2016. This application simply seeks to amend the access arrangements to the bridge deck from either side of the railway line. All other aspects of the scheme remain as approved.

6.2.2 Overall, the replacement bridge will improve connectivity to and from the town centre from the railway station, enhancing the experience at the point of arrival in the town centre by rail which is considered to be of significant benefit.

#### **6.3 Visual Impact**

6.3.1 The proposed alterations will result in changes to the appearance of the approaches to the approved bridge structure. The most significant change will be to the south east side of the railway line (alongside platform 2) where the proposed access ramp will no longer be provided. This would have involved the provision of a significant engineered embankment, extending 85m in length and raising to a maximum height of approximately 7 metres. This aspect of the scheme would have formed a substantial feature in the area immediately alongside the A442 and railway line therefore its omission will ensure a more open aspect to this part of the development site.

6.3.2 The amended proposal will see the 'drum' area in between the two bridge decks accessed by a steel staircase comprising three flights and two landings which will open onto a smaller raised area to the rear of Platform 2. Precise details of the materials, design and colour of the staircase and handrails will need to be submitted as a condition of the planning approval but the indicative

elevations suggest this aspect of the scheme will respect the character and appearance of the approved bridge design.

- 6.3.3 The alterations to the north-east side of the railway line (Platform 1) will see the existing ramped approach utilised instead of providing a new retaining structure. The ramp will connect to the new bridge via a 31m steel bridge supported by piers. This also avoids the need to encroach into the adjacent woodland areas. The staircase access to/from Platform 1 will be provided by a steel staircase accessed directly from the north east end of the bridge landing area. As with the amendments to the platform 2, the proposal will ensure a more open aspect within the station, increasing natural light and enhancing the character and feel upon arrival at the station by train.
- 6.3.4 Landscaping: As with the original approval, a full hard and soft landscaping scheme will be provided in order to complement the proposed footbridge structure, however, as no details are available at this stage, this will be requested as a condition should planning approval be granted.
- 6.3.5 The amended proposals will not affect the appearance of the main bridge deck and span which remains as approved. In visual terms, the omission of retaining structures and embankments will be of benefit to the station and the revised staircase, ramps and bridge links will respect the scale and appearance of the overall development. The application therefore continues to comply with all relevant local plan policies and national guidance.

#### **6.4 Access Issues**

- 6.4.1 The applicant advises that the revised design continues to significantly improve accessibility for all users, whilst working with the physical constraints of the bridge's location, by: -
- Providing the first lift accessible station in the borough;
  - Continuing to provide ramped access to the bridge from the north east side, providing step free access for pedestrians, cyclists and the National Cycle Route 55.
  - Reducing the gradient of the bridge to 1:20, compared to the current main span gradient of 1:17;
  - Providing 'wheelramps' within the staircases on both sides of the railway line for use by cyclists using the train station;
  - Providing a more open, visible structure and approaches, including lighting, improving the user experience especially outside of daylight hours.
- 6.4.2 The proposed amendments will affect the way the bridge is accessed from both platforms at the railway station but will not impact upon access arrangements at the south west end onto Ironmasters Way and Rampart Way.
- 6.4.3 At its north eastern end, ramped access will continue to be provided, utilising the ramped approach to the existing bridge which will link to the new bridge landing via a 31m steel bridge. This will ensure that a continuous connection to the National Cycle Route (NCR) 55 is maintained. A new steel staircase at

the end of the bridge deck will carry the majority of pedestrians travelling to and from Platform 1 but for those users that cannot use stairs, the customer lift and ramp will offer an alternative option. Officers are satisfied that this arrangement continues to offer a range of options to cater for a range of customer access requirements.

- 6.4.4 At its south western end, the bridge will continue to provide level access to Ironmasters Way and the town centre beyond and also to Rampart Way via a newly provided footpath.
- 6.4.5 Impact upon Public Rights of Way Network: Currently, the existing bridge provides access for pedestrians and cyclists coming to and from the Town Centre, Priorslee and Stafford Park, and Central Park. The bridge is, therefore, a key link in the strategic rights of way and cycle route network in the Borough. Access from the Town Centre is directly from the path leading from Ironmasters Way and access from Stafford Park and Priorslee is via a ramp leading directly from the station approach as shown on the 'Existing Access Routes to Be Removed' Plan. Two routes lead to the bridge from Central Park, running either side of the A442. Access to the bridge from both paths is via steps on the north west side or a ramp on the south-east side. Whilst the existing bridge is used by people in wheelchairs and mobility vehicles as well as by people pushing buggies and pushchairs, the gradients in the existing ramps exceed that which is currently required to make the structure Equalities Act compliant.
- 6.4.6 The proposed bridge will continue to provide level access from the Town Centre at the south west end and the proposed ramped access at the north east end will provide an Equalities Act compliant alternative to the current route. This will ensure continuous access is provided for the NCR 55 which crosses the bridge and will not disadvantage any user accessing the new structure from these points.
- 6.4.7 The provision of an Equalities Act compliant route from the Town Centre to Central Park will be established without using the new bridge, through the provision of a pelican crossing across Rampart Way, utilising a new footpath link from the south-west bridge approach. Access to Central Park can also be obtained via the bridge by using the path along the north-east side of the A442, using the flight of stairs or customer lift to be provided. To achieve an Equalities Act compliant route from Priorslee and Stafford Park to Central Park, users will need to cross the bridge and utilise the Silkin Way route described above. Whilst this route is slightly longer than the present route, this minor inconvenience is more than mitigated by that route being Equalities Act compliant whereas the existing ramp is not. These routes have been shown in the submitted plan reference EI277 – 101 Rev 0. Officers are therefore satisfied that suitable access options are available for wheelchair and mobility vehicle users, people pushing pushchairs and buggies using the PROW route between the Town Centre, Railway Station/Stafford Park/Priorslee and Central Park and that significant benefits are provided in the provision of a bridge facility with Equalities Act compliant route options.

- 6.4.8 The Council's Rights of Way Officer is therefore satisfied that the proposed new access provisions will provide an enhanced, or not substantially less convenient alternative, following the omission of the existing staircases and ramps currently available. As a result, it is not considered that users of the PROW would be unduly affected or disadvantaged as a result of the proposed amendments. The attached plan provides a visual overview of the current and proposed access provisions.
- 6.4.9 Access Arrangements at Railway Station: The omission of the access ramp linking the bridge with Platform 2 may give rise to more significant concerns for those rail passengers arriving at Platform 2 who are unable to use the staircase provided. The applicant states that the bridge lift will be available for use by all customers arriving at Platform 2. Officers have queried what contingency arrangements will be in place at those times when the lift is out of service. Discussions have taken place with the applicant to consider solutions and it has been suggested that provisions can be made to pick up passengers from Platform 2 using the proposed Grasscrete maintenance track. This track would primarily be intended for use by maintenance vehicles to access the bridge lift and would therefore be secured by bollards but could also be used as a means of providing vehicular access to Platform 2 should it be required in the event of lift failure or any emergency. The principle of the use of this track as an emergency access has been agreed in principle with the Highways Team, however, precise details of the layout and construction of the service track will need to be submitted as a condition of any permission granted.
- 6.4.10 It is acknowledged that the lift will be available as a suitable alternative to the flights of stairs provided to access the bridge deck from Platform 2. The applicant advises that the lifts could be available 24 hours a day, subject to final agreement with Network Rail and London Midland, although as a minimum the lifts will be operated on a 'first and last train basis' being operational before the first train of the day and operating after the final departure of the day from the station (currently 05.38 am first departure and 00.36pm last departure). A Management Plan will be put in place with Network Rail, detailing the arrangements for the routine and emergency operation of the lifts. This information has not been submitted with the application but will be requested as a condition should planning approval be granted.
- 6.4.11 On the understanding that alternative access arrangements can be agreed by condition and a suitable management plan can be agreed and secured with the operators of the station, officers are satisfied that the proposed access arrangements will be acceptable and the proposed development, as amended, will continue to provide an enhanced pedestrian link between Telford Central Station and Telford Town Centre, meeting the requirements of CTAAP Policy TC7.

## **6.5 Trees and Ecology Issues**

- 6.5.1 The application has been accompanied by an Addendum to the original Environmental Report which considers the various environmental issues

associated with the proposed amendments. The report takes into consideration ecological impacts and the likely impact upon trees.

- 6.5.2 With regard to Trees, the report identifies that the amended scheme will involve a smaller footprint than the previous approval and would no longer require encroachment into areas of woodland to the north of the existing bridge. Overall, the proposal will involve the removal of less vegetation. It is noted, however, that the proposal will require an additional laydown area during the construction period to be situated to the north of the bridge, in between the A442 and railway line. Clearance of this area has already taken place under the supervision of the Councils Trees Officer and no objection is raised to the use of this area. Some of the areas no longer to be incorporated have already been cleared and these will be the subject of a replacement planting plan which is proposed as a mitigation measure in the addendum report and can be agreed as part of the Landscaping Scheme to be submitted.
- 6.5.3 With regard to ecological impacts, the Addendum Report concludes that proposed changes to the bridge design will not affect the results of the original ecological surveys or the minimal effects identified, other than the requirement to remove less vegetation. The Council's Planning Ecologist has inspected the Addendum report and is satisfied that the findings of the previous Ecological Surveys continue to apply and is satisfied that the additional 'laydown area' identified to the north of the bridge had been subject to relevant surveys before being cleared. On this basis, no objection is recommended, subject to conditions requiring further checks for Badgers prior to commencement and that site clearance takes place outside the bird nesting season. Conditions requiring an Ecological Method Statement, landscaping plan and external lighting plan are also required.

## **7.0 CONCLUSIONS**

- 7.1 The amended proposals will not materially alter the appearance of the main bridge deck structure which remains as approved, and in visual terms, the omission of retaining structures and embankments will be of benefit to the overall design, reducing the scale and massing of the bridge approaches and maintaining the open aspect at the station.
- 7.2 In terms of access issues, officers are satisfied that users of the Public Rights of Way network will not be unduly disadvantaged by the proposal and that suitable contingency plans are in place to ensure that rail passengers will be catered for in the event that the Platform 2 lift is out of order. Subject to conditions securing maintenance arrangements and contingency plans with the station operators and owners and details of suitable emergency taxi pick-up areas being submitted, the proposed amendments to the ramps and approaches are considered acceptable.
- 7.3 The proposal raises no additional ecological issues and will no longer encroach into an area of woodland to the north of the Platform 1. An additional laydown area to the north of the bridge has been cleared under the supervision of the Councils Trees Officer and a replacement tree planting

scheme and landscaping scheme will be secured as a condition of the planning permission.

- 7.4 Overall, the proposed replacement footbridge continues to comply with local planning policy which seeks to enhance the quality of pedestrian and cycle links from the Town Centre to Telford Central Station. The design continues to be of a high quality, providing an iconic feature in a key gateway site and arrival point into the town centre. The existing bridge will remain open until the new bridge is brought into use thus ensuring continuous connectivity between the town centre and railway station during the construction period.
- 7.5 The proposal has been assessed against and is considered to comply with policies contained within the local development plan and national planning guidance and is recommended for approval.

## **8.0 RECOMMENDATION**

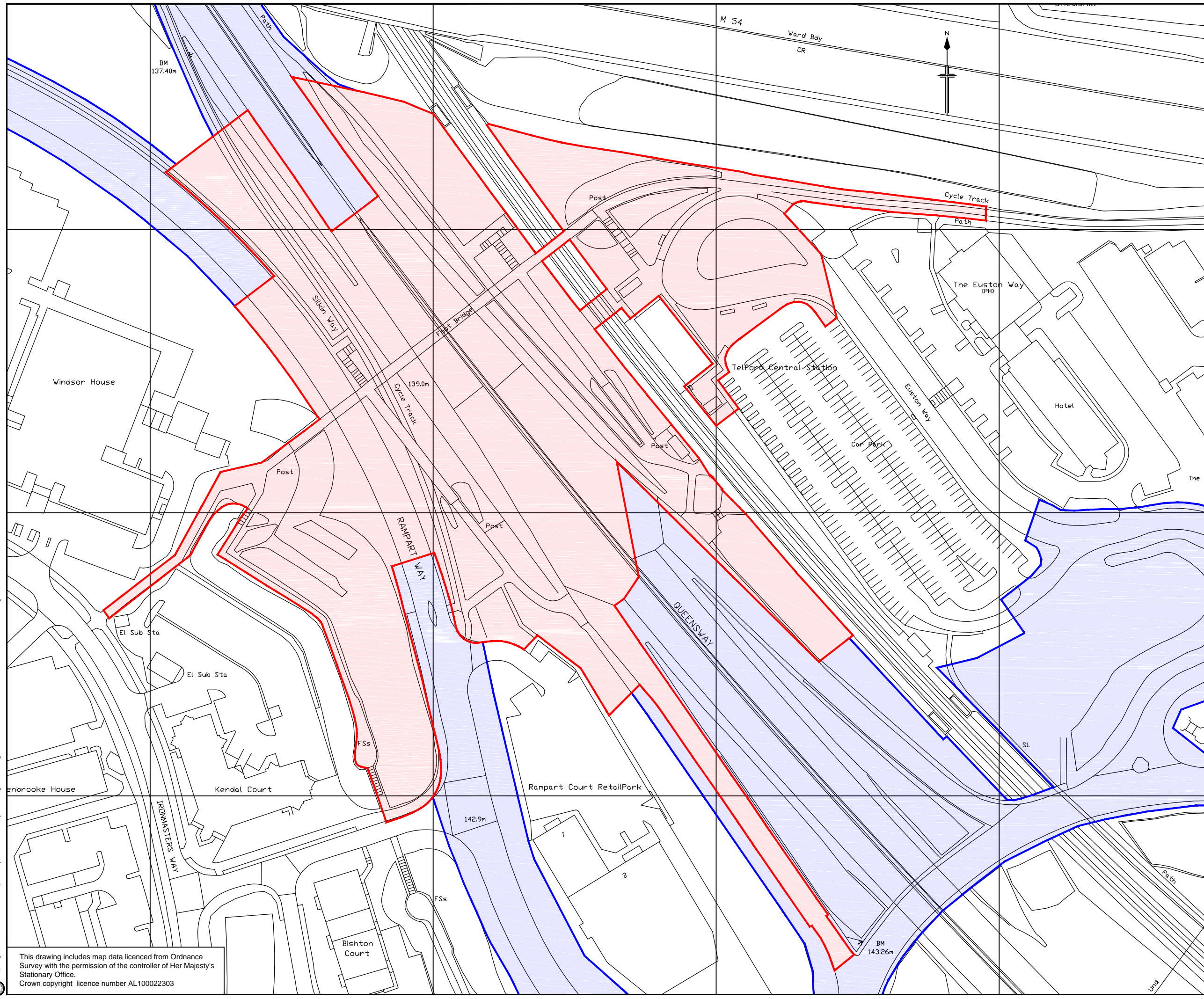
- 8.1 Based on the conclusions above, the recommendation to the Planning Committee on this application is that **DELEGATED AUTHORITY** be granted to the Development Management Service Delivery Manager to **GRANT PLANNING PERMISSION** subject to the following conditions and informatives (with authority to finalise conditions and reasons for approval to be delegated to Development Management Service Delivery Manager):

### Conditions

1. A04 Time limit
2. B149c Pre-commencement Badger Inspection
3. B010 Details of Materials
4. B029c Details of staircase, ramps, etc
5. B031 Emergency Access to platform 2
6. B049c Traffic Management Plan
7. B061 Surface Water Drainage
8. B121 Landscaping Design
9. B150 Construction Management Plan
10. B141 Ecological Method Statement
11. B145 External Lighting Strategy
12. Bcust Public Art details
13. Bcust Emergency Access Plan – Platform 2
14. Bcust Maintenance plan
15. C089c Tree Report Recommendations
16. Ccust Bridge removal
17. C38 Approved Plans

### Informatives

- I11 Highways
- I25b Nesting Birds
- I40 Conditions
- I41 Reason for Grant
- RANPPF1 Approval - National Planning Policy Framework.

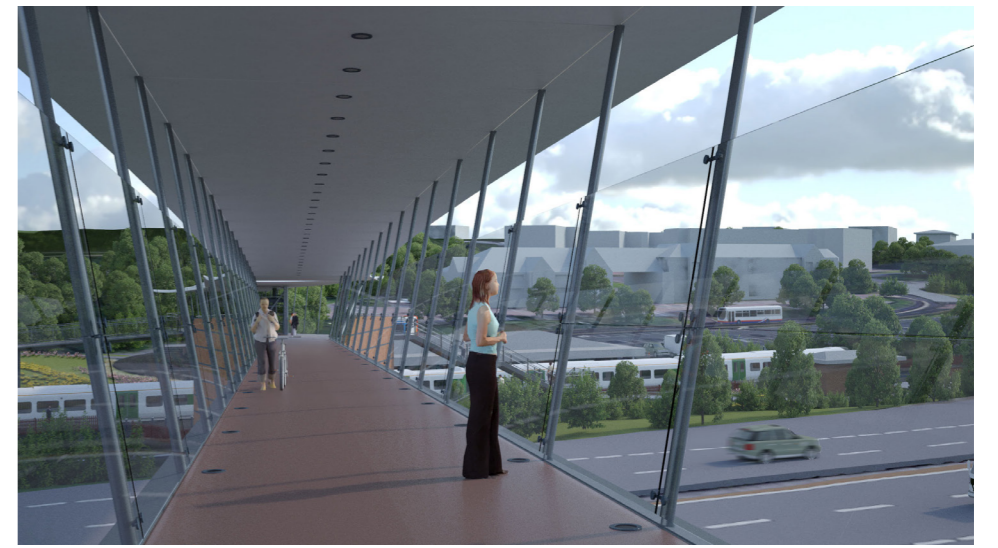
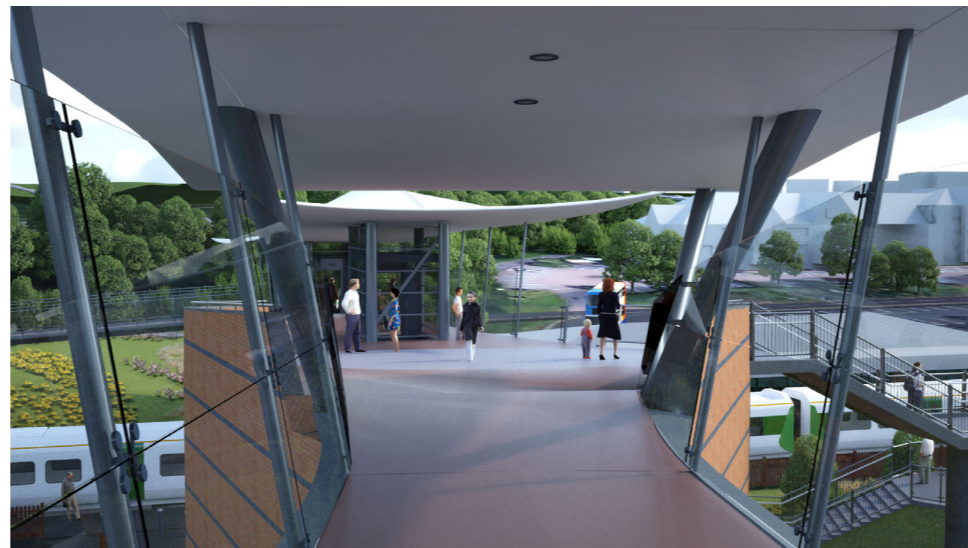
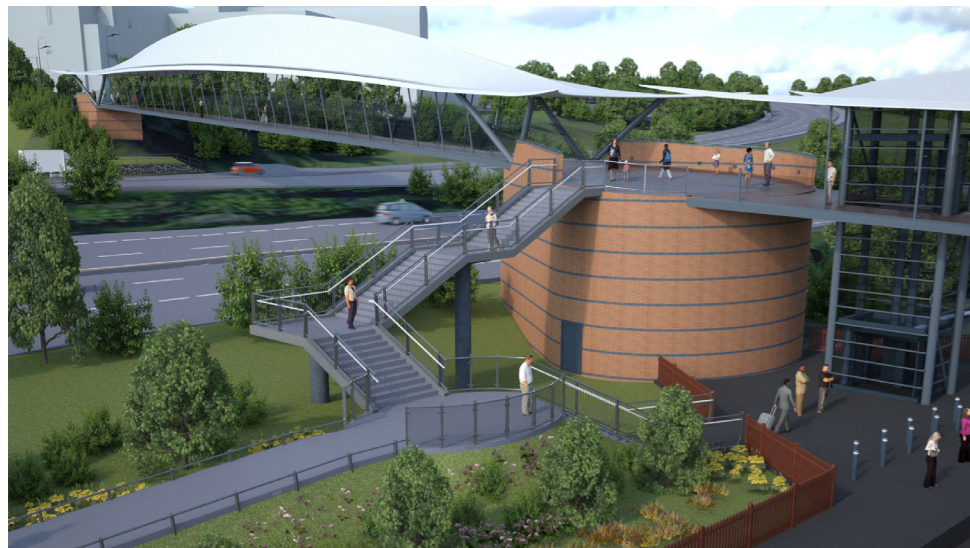
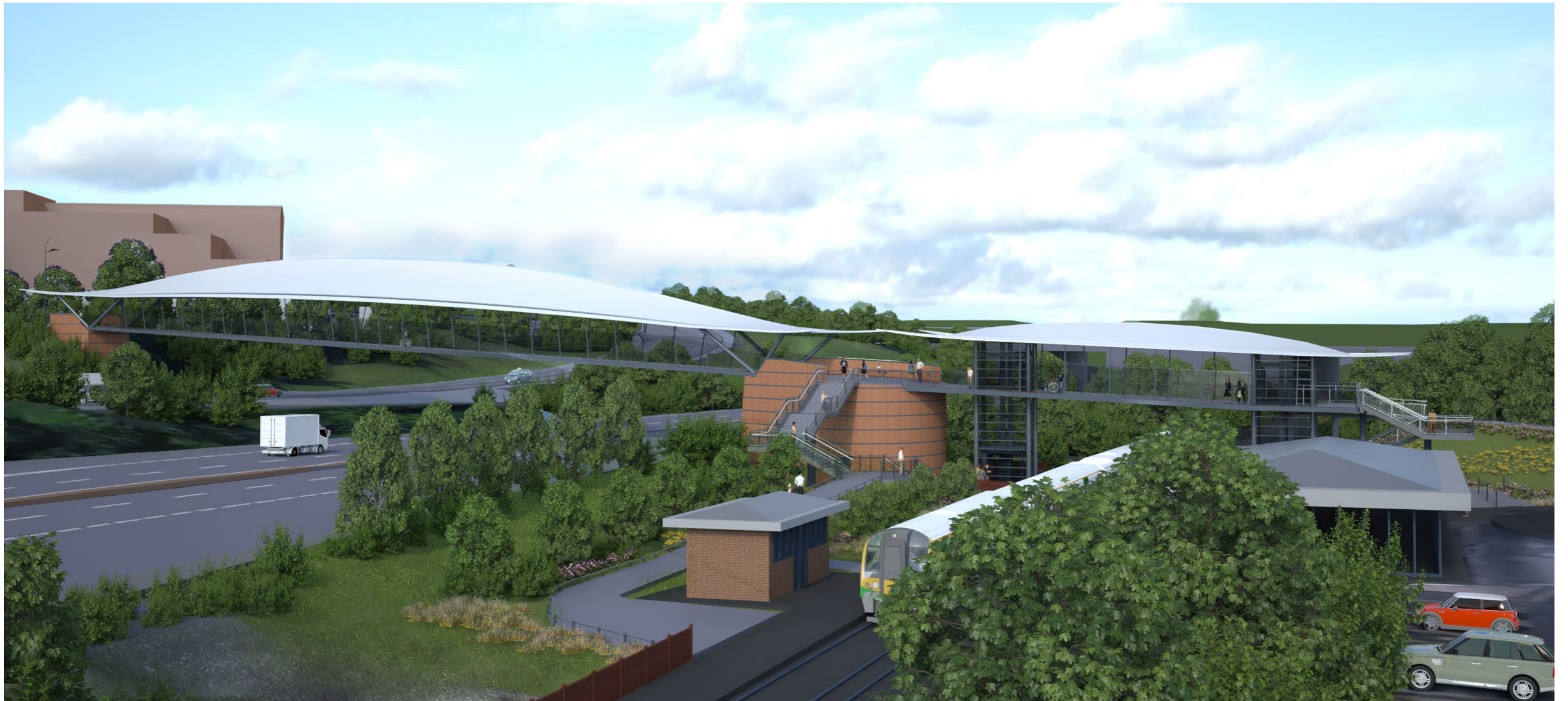


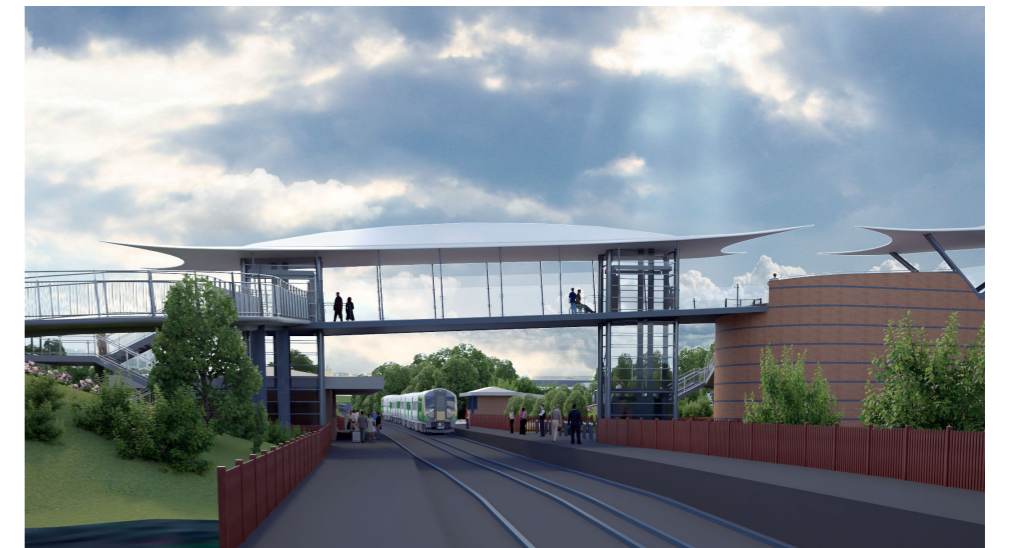
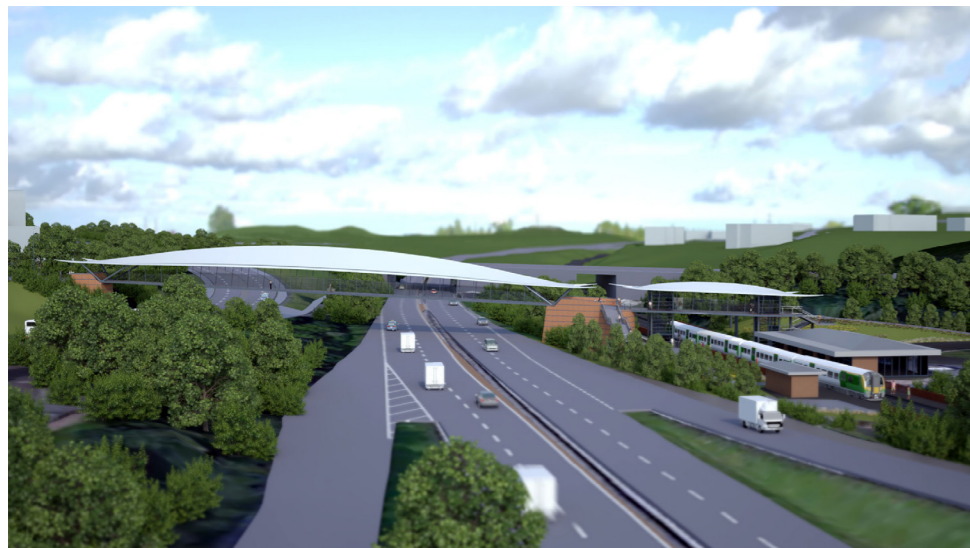
- Key**
- Application Boundary
  - Telford and Wrekin Council Land Ownership Boundary

P:\Progression\B224TW09 - Rampart Way & Hill Park Way\01 - Bridges\Planning\Issue to Stakeholders\B224TW19-PA-100-01 REV 0.dwg - 05/07/2016 09:28:18 - A3-VFrame - JonesDH

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0	29/06/2016	FOR PLANNING	DJ	D.J.L	SE	D.J.L
Rev	Rev. Date	Purpose of revision	Drawn	Checked	Rev'd	Appr'd
 Jacobs House, Shrewsbury Business Park, Silka Drive, Shrewsbury SY2 6LG Tel +44(0)1743 284800 www.jacobs.com						
Client						
Project TELFORD TOWN CENTRE PROJECT FOOTBRIDGE REPLACEMENT						
Drawing title SITE LOCATION PLAN						
Drawing status FOR PLANNING						
Scale			1:1250 @ A3		DO NOT SCALE	
Jacobs No.			B224TW19			
Client no.			-			
Drawing number B224TW19/PA/100/01						Rev 0
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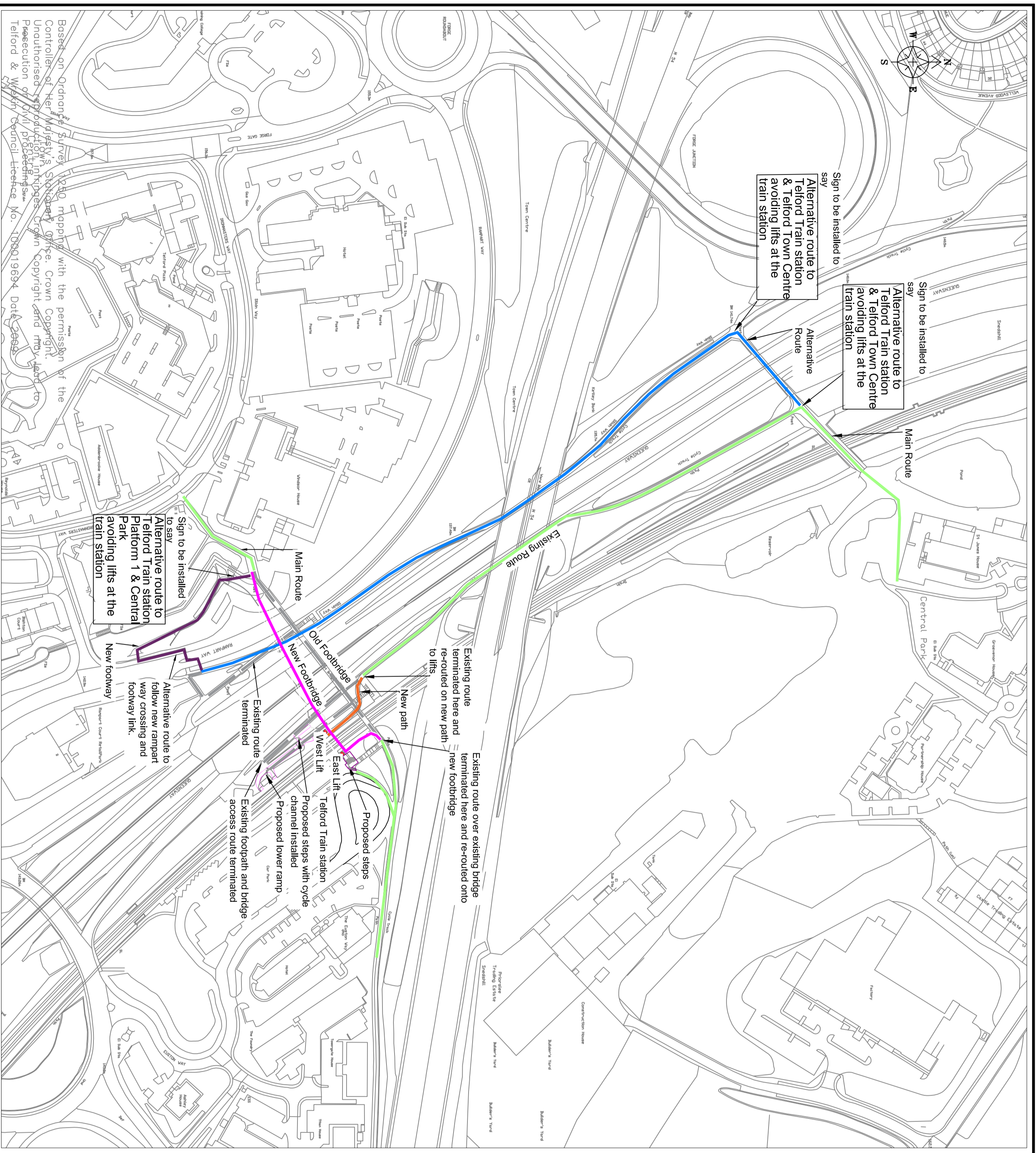








- Key**
- Bridge Crossing
  - Access Ramp
  - Access Stair
  - Access Lift
  - National 55 Cycle Route
  - Local Cycle Route
  - Public Footpaths



**Key**

- Main route
- Alternative route - avoiding lifts
- Existing routes terminated under project
- New footbridge
- New crossing and ramped access
- New footway link to platform and lift
- ◆ New lift

**Note:**  
 A grasscrete maintenance track is to be installed to access the central plant rooms and lifts. In an emergency, this access route may be used to transport the disabled or injured.

Rev	Issue	File Type	Destination	Date	Approved
A	06/04/17	Text revised	SCN		

Jonathan Rowe, Director of Neighbourhood, Customer & Cultural Services, Darcy House, Telford, TF3 4JA

**Telford & Wrekin COUNCIL**

**TELFDOR CENTRAL FOOTBRIDGE REPLACEMENT**

Drawing title  
**PROPOSED FOOTWAY CYCLEWAY ROUTES**  
**TRAFFIC MANAGEMENT**

Scale 1:2000 A2 Date 5/04/2017 Drawn by SCN Checked by

Drawing number **EI277 / 100** Rev **A**

INFORMATION

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## INFORMATION RECEIVED SINCE PREPARATION OF REPORT

Application number	TWC/2017/0208
Site address	Foot Bridge to Telford Central Railway Station, Telford, Shropshire
Proposal	Minor amendment to planning permission reference TWC/2016/0641 (Erection of replacement footbridge following demolition and removal of existing bridge structure together with associated level alterations, provision of retaining structures and access alterations) to omit pedestrian access ramp and supporting earthworks and amend the design of remaining ramps, staircases and supporting structures
Recommendation	Full Grant

### 1.0 ADDITIONAL REPRESENTATIONS

#### 1.1 Additional Agent Comments:

To correct points made in the officer's report to Plans Board as follows: -

- 1.2 Para 6.4.1: Refers to cycle chutes being provided in staircases on both sides of the railway line. In reality, only the staircase on the platform 2 (westbound) side will contain the cycle chute. Given the provision of a ramp on the platform 1 (eastbound) side, cycle chutes were not considered to be necessary.
- 1.3 Para 6.4.9: Refers to access to the emergency/maintenance track being secured by bollards. Whilst this detail is yet to be agreed, additional submitted plans show gates and fences being provided to secure this area; a pedestrian one adjacent to the central 'drum' and double width vehicular gates off the A442.
- 1.4 Para 6.5.2: Refers to 'some areas no longer incorporated already having been cleared' of trees and vegetation. This was in particular reference to the area of woodland to the north of Platform 1 which was shown to contain a looped access ramp under the original approved scheme. As the existing ramped approach is now being retained there is no longer a need to provide the looped ramp. Officers incorrectly reported that the area of woodland had been cleared. Following clarification by the agent, I am happy to clarify that the area to the north of the existing ramped approach adjacent Platform 1 has not been cleared. Officers are satisfied that no trees have been removed other than those needing to be to allow construction of the current proposals.
- 1.5 Network Rail comment:  
Confirms that Network Rail would have no objections to the provision of a grasscrete 'green' track providing the maintenance access route to the platform 2 lift area, and its possible use in the rare emergency situation when the platform 2 lift is broken and a disabled passenger gets off the train.

- 1.6 Public Comment:  
One additional public comment questioning how cyclists and pedestrians can access the bridge on approach from the cycle/pedestrian path from Oakengates which is a well-used path.
- 1.7 To clarify, the footpath from Oakengates will divert onto the rear of Platform 2 where pedestrians will be able to use the lift of staircase o the bridge deck.

## 2.0 CONCLUSION

- 2.1 On the basis of the above additional representations and comments, there is no change to the recommendation which remains that permission be granted, subject to conditions and informatives.
- 2.2 Officers also wish to seek extra delegated authority for the Planning Service Delivery Manager to approve any minor variations to the public rights of ways plan which may be required as a result of any issues occurring during the diversion/extinguishment orders which needs to be carried out as part of the development process.

## 3.0 RECOMMENDATION

- 3.1 The recommendation to the Planning Committee on this application is that **DELEGATED AUTHORITY** be granted to the Development Management Service Delivery Manager to **GRANT PLANNING PERMISSION** subject to the following conditions and informatives (with authority to finalise conditions, reasons for approval and any minor variations to the public rights of ways plan required as a result of any issues occurring during the diversion/extinguishment orders to be delegated to Development Management Service Delivery Manager):

### Conditions

1. A04 Time limit
2. B149c Pre-commencement Badger Inspection
3. B010 Details of Materials
4. B029c Details of staircase, ramps, etc
5. B031 Details of Maintenance Access track
6. B049c Traffic Management Plan
7. B061 Surface Water Drainage
8. B121 Landscaping Design
9. B150 Construction Management Plan
10. B141 Ecological Method Statement
11. B145 External Lighting Strategy
12. Bcust Public Art details
13. Bcust Emergency Access Management Plan
14. Bcust Lift Maintenance plan
15. C089c Tree Report Recommendations
16. Ccust Existing bridge open and retained until new bridge in use
17. C38 Approved Plans

Informatives

I11 Highways

I25b Nesting Birds

I40 Conditions

I41 Reason for Grant

RANPPF1 Approval - National Planning Policy Framework.